

DESIGN AND ACCESS STATEMENT

PROPOSED APARTMENT COMPLEX,
FORMER GLAMORGAN HOLIDAY HOTEL,
THE SQUARE,
PORTHCAWL,
SOUTH GLAMORGAN,
WALES,
CF36 3BW

Job Ref:

184017-IDL-01-XX-DR-A-R0070-S3-P02

28th February 2020

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1.0

INTRODUCTION

- 1.1 DOCUMENT SCOPE
- 1.2 THE BRIEF

1.0 INTRODUCTION

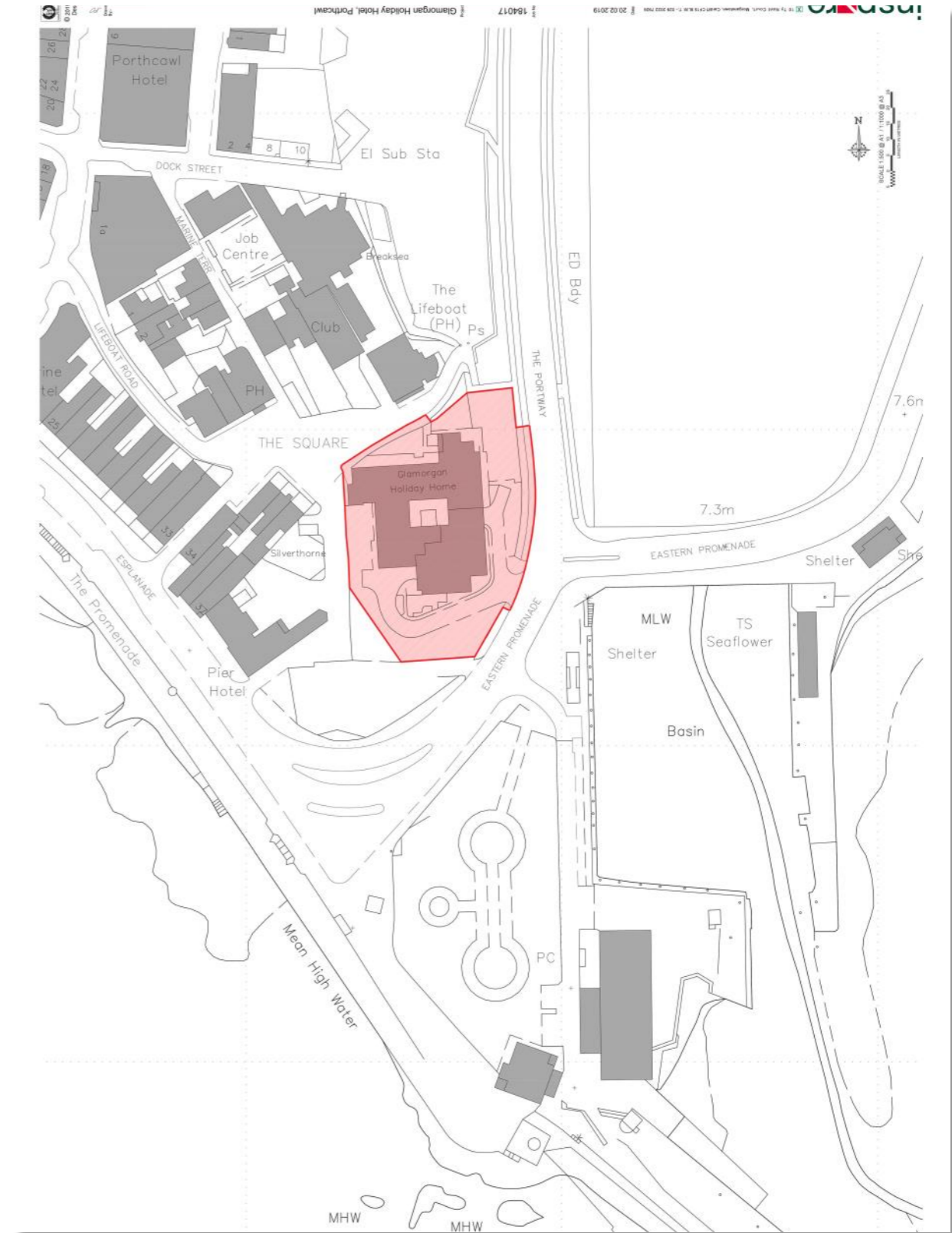
1.1 DOCUMENT SCOPE

This Design and Access Statement (DAS) has been prepared by Inspire Design on behalf of Xcape Limited to support an application for full planning permission for an apartment complex.

The development comprises of 54 No residential dwellings with associated facilities . Of the 54 dwellings 16 No are to be affordable properties representing 30%.

The proposal will use a new vehicular and pedestrian access point off The Portway serving the car park and the affordable housing. A new pedestrian access point dedicated for the open market apartments is located off The Square.

This design and access statement should be read in conjunction with the drawings outlined in the following page, along with all other reports submitted in support of the application.



Site location plan

1.0 INTRODUCTION

1.1 DOCUMENT SCOPE

The proposed development is located in Porthcawl, Wales, on a brown field presently occupied by the vacant Glamorgan Holiday Hotel.

The proposed residential development includes:

- 38 open market apartments (mix of 1, 2 and 3 beds)
- 16 affordable apartments (mix of 1 and 2 beds)
- Refuse Stores and Cycle Stores
- Entrance Lobbies
- Lift Access
- Landscaped amenity spaces
- 64 No car parking spaces

The development proposals are the result of a design process which has included context analysis, technical studies, identification of constraints and opportunities.

This DAS follows the structure of:

Assessment - providing a review of the local area and site analysis.

Evaluation - demonstrating how the analysis has shaped the design

Design - Setting out the proposal for the design

The structure of this Design and Access Statement is based on the CABE document: 'Design and Access Statements: How to write, read and use them'.

The purpose of this statement is to outline the design process and decision making which has informed the proposed scheme seeking approval.

This statement aims to demonstrate that the principles of inclusive design and access have been considered from the outset of the development process.

This proposal and the supporting documents are aimed at creating a distinctive and accessible development of high quality, which responds to the setting of the site.

The following architectural drawings form part of the planning submission.

Document number	Description
184017-IDL-01-XX-DR-A-P10001-S3	Location plan
184017-IDL-01-ZZ-DR-A-P10002-S3	Existing site plan
184017-IDL-01-ZZ-DR-A-P20024-S3	Proposed Site Plan (Lower Ground Floor)
184017-IDL-01-ZZ-DR-A-P20025-S3	Proposed Site Plan (Ground Floor)
184017-IDL-01-ZZ-DR-A-P20026-S3	Proposed Site Plan (Roof Plan)
184017-IDL-01-B1-DR-A-P20027-S3	Proposed Lower Ground floor
184017-IDL-01-GF-DR-A-P20028-S3	Proposed Ground floor
184017-IDL-01-01-DR-A-P20029-S3	Proposed First floor
184017-IDL-01-02-DR-A-P20030-S3	Proposed Second floor
184017-IDL-01-03-DR-A-P20031-S3	Proposed Third floor
184017-IDL-01-04-DR-A-P20032-S3	Proposed Roof
184017-IDL-01-XX-DR-A-P20037-S3	Proposed Elevations
184017-IDL-01-XX-DR-A-P20038-S3	Proposed Elevations
184017-IDL-01-XX-DR-A-P40039-S3	Proposed Site Section
184017-IDL-01-XX-DR-A-P60042-S3	Perspective view
184017-IDL-01-XX-DR-A-P60043-S3	Perspective view
184017-IDL-01-ZZ-DR-A-P20126-S3	Proposed landscape plan

1.0 INTRODUCTION

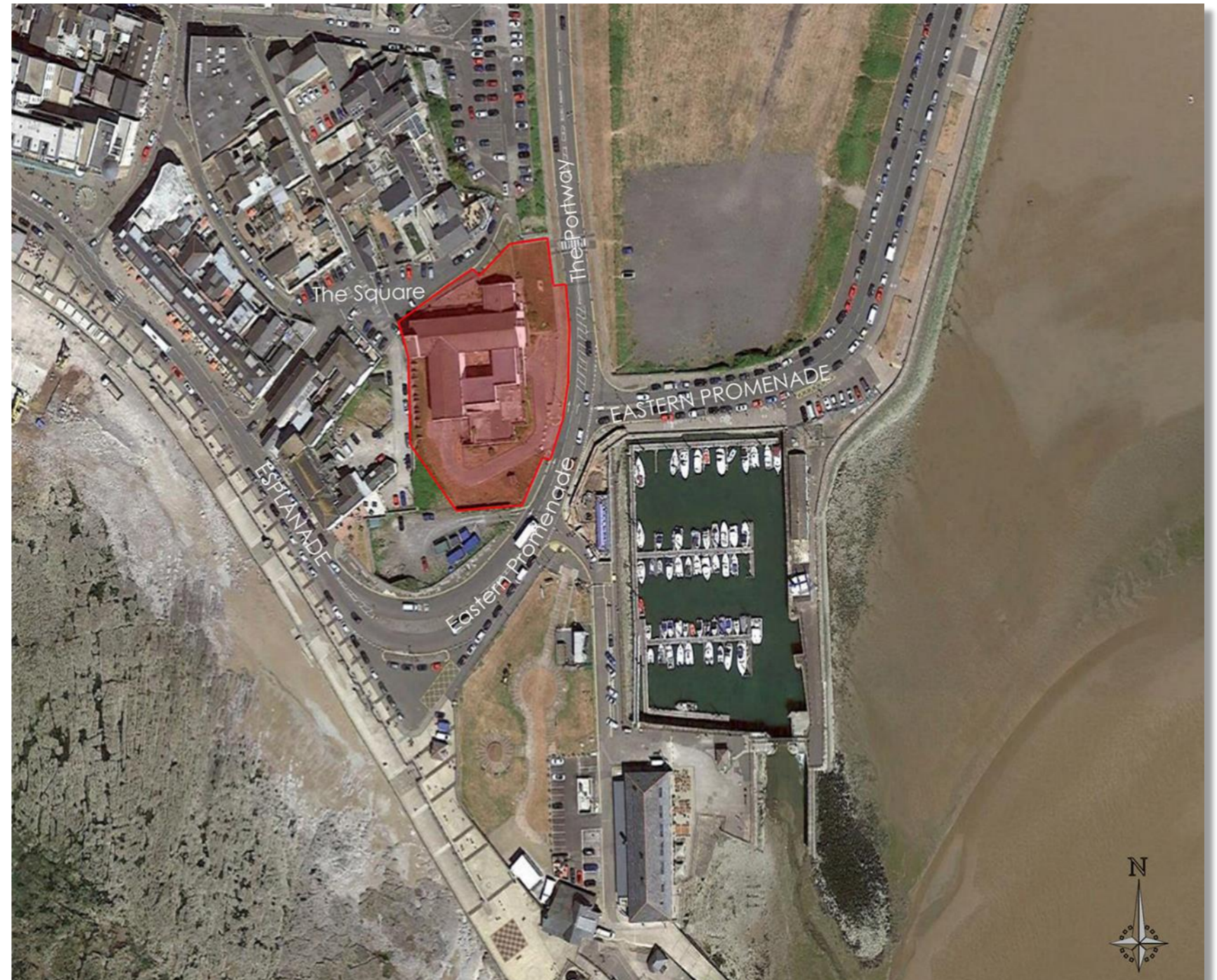
1.2 THE BRIEF

Inspire Design have been appointed by Xcape Limited to undertake the architectural services relating to the proposed scheme including but not limited to master planning of the site, design of the residential building and car park.

The proposed residential development has to include:

- a range of one, two and three bedroom open market apartments,
- a range of one and two bedroom affordable apartments,
- refuse Stores and Cycle Stores
- entrance Lobby
- lift access
- landscaped amenity space
- car parking

The design objective is to include the above in an attractive and efficient design that integrates into the existing urban environment assisting to fulfill the town's aspiration for the renovation of this part of Porthcawl. The scheme should take advantage of its location and take advantage of the distant views over the Bristol Channel.



Aerial view showing location of application site.

2.0

ASSESSMENT

- 2.1 SITE LOCATION
- 2.2 PHYSICAL CONTEXT
- 2.3 SOCIAL & ECONOMICAL CONTEXT
- 2.4 POLICY CONTEXT AND INVOLVEMENT

2.0 ASSESSMENT

2.1 SITE LOCATION

The application site is located in Porthcawl, Wales.

The site falls within the administrative boundary of the Bridgend County Borough Council (BCBC).

The address of the application site is:

FORMER GLAMORGAN HOLIDAY HOTEL,

THE SQUARE,

PORTCAWL,

SOUTH GLAMORGAN,

WALES,

CF36 3BW

Porthcawl is a town and community on the south coast of Wales in the county borough of Bridgend, 29 miles west of the capital city, Cardiff and 21 miles southeast of Swansea.

Historically part of Glamorgan and situated on a low limestone headland on the South Wales coast, overlooking the Bristol Channel, Porthcawl developed as a coal port during the 19th century.

The historic harbour with its old Lighthouse, Watchtower and Lifeboat Station used to be a busy exporting port and fishing harbour before the huge docks at Cardiff, Barry, Port Talbot and Swansea eclipsed them in size and capabilities.

DESIGN AND ACCESS STATEMENT | Apartment complex, The Square, Porthcawl, Wales.

Following the closure of the docks in 1906, it became known as a holiday resort.

Porthcawl has an extensive promenade that runs along the seafront from Lock's Common in the west to the harbour (Esplanade), before joining the eastern promenade and leading to Coney Beach and Griffin Park. Since the closure of the railway station in 1965, Porthcawl's main link is via the road network system.

From the harbour the existing road called The Portway leads north, through the Portway roundabout, to the A4106 (Boulevard St Sebastian sur Loire) that is the main link to reach Bridgend and the motorway M4.

The site is located at the junction between The Portway Road and The Eastern Promenade, within the southern most extent of the town within easy walking distance to shops, local attractions and the like.

It is in close proximity to the west quay of Porthcawl harbor and at the beginning of the Esplanade, which continues westwards towards Rest Bay.



View of the existing Glamorgan Holiday Hotel from the junction between the Eastern Promenade and The Portway



Site location diagram



Site location, Porthcawl Conservation Area and listed buildings

2.0 ASSESSMENT

2.1 SITE LOCATION

Porthcawl, like many British resorts, has suffered a decline in its holiday trade over recent years, especially since most of the South Wales Valleys coal pits have closed. To overcome this Porthcawl waterfront is proposed for substantial regeneration as part of the "7 Bays Project".

A Supplementary Planning Guidance (SPG) has been developed to provide guidance for the redevelopment of Porthcawl's Waterfront and was adopted to support the Bridgend Unitary Development Plan (UDP) by the Council at its Planning and Development Committee held on the 1st November 2007. The Planning Guidance outlines proposals that will result in the comprehensive regeneration of Porthcawl's waterfront, stretching from Cosy Corner and the harbour in the south, to Trecco Bay in the east. The plan includes the construction of new sea defences, enabling regeneration of the area to take place and also protecting more than 440 existing properties from flood risk.

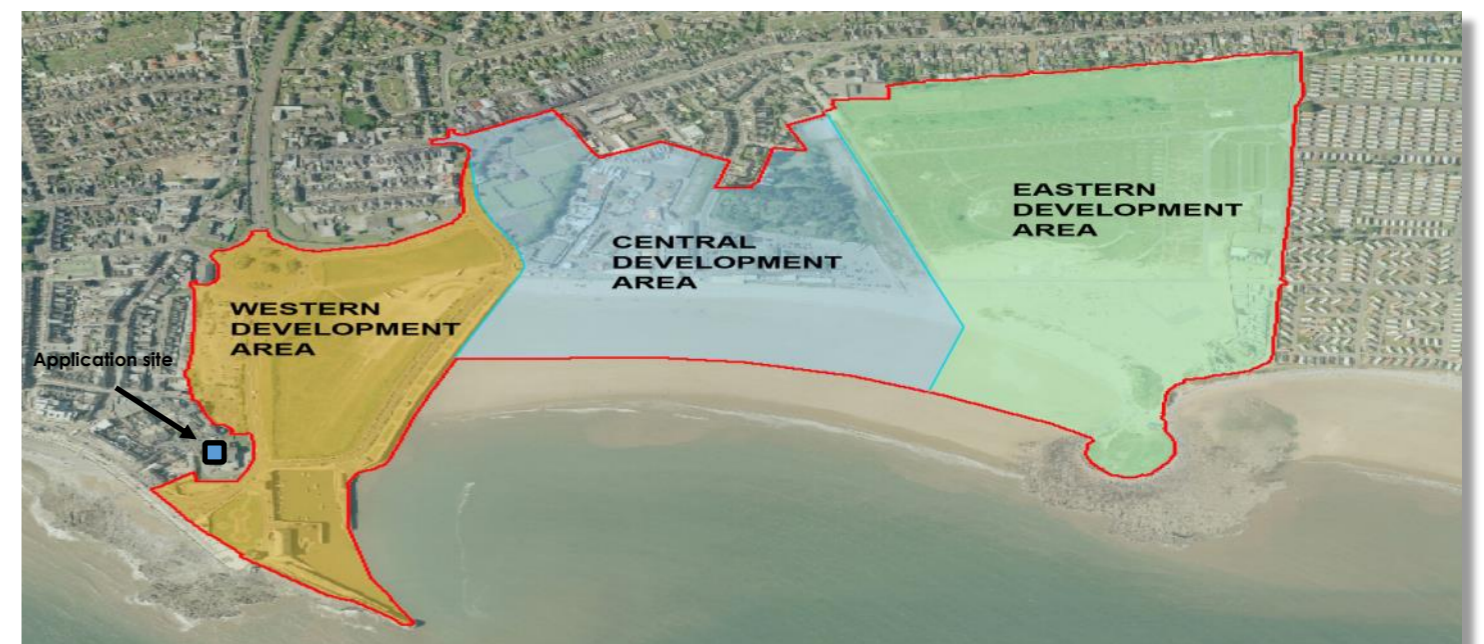
"The regeneration of the waterfront is a key element in the future development of Porthcawl as a premier seaside resort. This will be achieved through a combination of quality design of buildings and the surrounding environment, comprising residential, leisure, cultural, tourism and retail uses. Central to this will be a revitalised harbour area incorporating the reuse of historic buildings, new leisure facilities and a fully refurbished harbour set within a prestigious and vibrant waterfront." (SPG) Vision.

The waterfront area has been divided in three development areas: western, central and eastern.

The proposed site sits adjacent to the Western Development Area and although it sits outside the Waterfront regeneration area, it is recognised that due to its proximity, it needs to contribute to the success of the regeneration process.



Porthcawl 7 Bays Project Area (extract from SPG)



The Porthcawl Waterfront Area (extract from SPG)

2.0 ASSESSMENT

2.2 PHYSICAL CONTEXT

The application site is accessed from the Eastern Promenade immediately south of the junction with the Porthway. The site is irregular in shape and occupies an area of approximately 0.37 hectares.

It is located in a prime location at the southern tip of the town centre adjacent to the Porthcawl harbour.

The site is currently occupied by the remains of a vacant hotel, the Glamorgan Holiday Hotel.

The hotel was opened in 1974 as a partnership of 3 local authorities. The hotel was later transferred into a dedicated charitable trust. In 2015, Cartrefi Cymru took full responsibility for the hotel. They took the opportunity to temporarily close the hotel to allow for refurbishment and business reorganisation. It reopened in August 2015, but due to lack of customers closed down the following year.

The hotel capacity was of 14 twin and 25 single rooms. With most rooms having wash facilities. There was also a self catering apartment and a sea view apartment.

The existing buildings, which are considered to have limited architectural merit and provide little contribution to the character and appearance of the street scene, are proposed for demolition.

The site is bound:

- to the east by the pedestrian walkway running along The Portway Road / Eastern Promenade;
- to the south by a vacant site that separates the site from the Esplanade and the Pier Hotel belonging to BCBC and earmarked for future development (Harbour View Site);
- to the north by a irregularly shaped opening in the urban fabric called The Square;
- to the west by a narrow lane linking The Square with the rear of the Pier Hotel.

To the north-east of the site is a large area of green which is now partially used for car parking with the Eastern Promenade and Sandy Bay beyond.

To the north-west of the site is Porthcawl town centre comprising of small retail units, holiday accommodation and Porthcawl Museum.

Its position near the junction between the Eastern Promenade and The Portway makes the site highly visible when approached from the north, the east and the south.

The western and northern elevations of the hotel face The Square enclosing this space and contribute to the creation of an intimate environment. The existing main pedestrian access to the hotel is located in this part of the site.

An unadopted lane leads from The Square to the Esplanade along the western boundary. Any development of the site will be a focal feature of this lane and will have an impact on the character of the lane and square.

The site is located within Porthcawl Conservation Area and there are a number of listed buildings located to the east and south-east of the site, which includes the prominent Jennings building, which is Grade II listed.

The site is generally flat with sharp changes in levels concentrated along the western (+3 metres) and northern boundary (varying from +0.5 metres to +3 metres) where a series of retaining walls hold up the upper ground.

All boundaries are defined by stone walls of varying heights.

The surrounding area is a mix of residential, commercial and leisure use classes all within an urban curtilage with a variety of characters including houses, hotel, blocks of flats of varying designs and architectural styles, from more traditional approaches to more modern propositions.

2.0 ASSESSMENT

2.2 PHYSICAL CONTEXT

Local building forms vary from two storey dwellings and group of flats concentrated towards the town centre to the north, to larger scale three or more storey apartment/hotel buildings along the Esplanade to the south west.

The character of the Esplanade is reasonably simple, with predominantly lightly coloured render buildings with a degree of similarity of eaves, ridge heights with occasional expressed gables.

Building frontages are articulated by bay windows, overhanging eaves and recessed balconies.

Architectural styles in this location vary from modified art deco (Seabank Hotel), arts-and-crafts and Victorian three-storey lodging houses, many of which now have commercial ground floors.

Other notable historic buildings on the seafront include the Grand Pavilion, Pier, Lighthouse and the Seabank Hotel.

The overall composition of this linear space is of a relatively calm unity of scale, height and massing, held

together by the predominant use of light coloured renders on buildings set below pitched roofs of slate and tile.

The frontage is punctuated with a number of notable buildings such as the Grand Pavilion, and, more recently, Esplanade House.

Immediately adjacent to the site and the Pier Hotel a vacant triangular shaped seafront site named Harbour View Site is promoted by BCBC for development.

In this same location but on the opposite side of the Eastern Promenade, adjacent to Porthcawl Harbour, a new building, the Porthcawl Maritime Centre, is under construction (Planning ref. P/16/373/FUL) adjacent to Porthcawl Harbour.

Porthcawl Maritime Centre will be a multi-use destination known as 'Harbourside'. The flagship venue will establish the Harbourside as a prime destination for all-weather and year-round maritime activities whilst providing business, educational, cultural, and health and well-being benefits to the south coast of Wales.

The new Maritime Centre will complement the

redevelopment of the adjacent Jennings building and continue the economic regeneration of Porthcawl.

The images on the following pages illustrate the character of the immediate site area in terms of its distinctive architectural features, material finishes, quality of spaces, and the scale, mass and height of buildings.

2.0 ASSESSMENT

2.2 PHYSICAL CONTEXT



1 Pier Hotel, Esplanade



9 Jennings Building, Porthcawl Harbour



2 Terraced properties, Esplanade



8 Breaksea Care Home



3 Esplanade House, Esplanade



4 Grand Pavilion, Esplanade



5 Sea Bank Hotel, Esplanade



6 Salt Water Inn, The Square



7 Varies properties, The Square

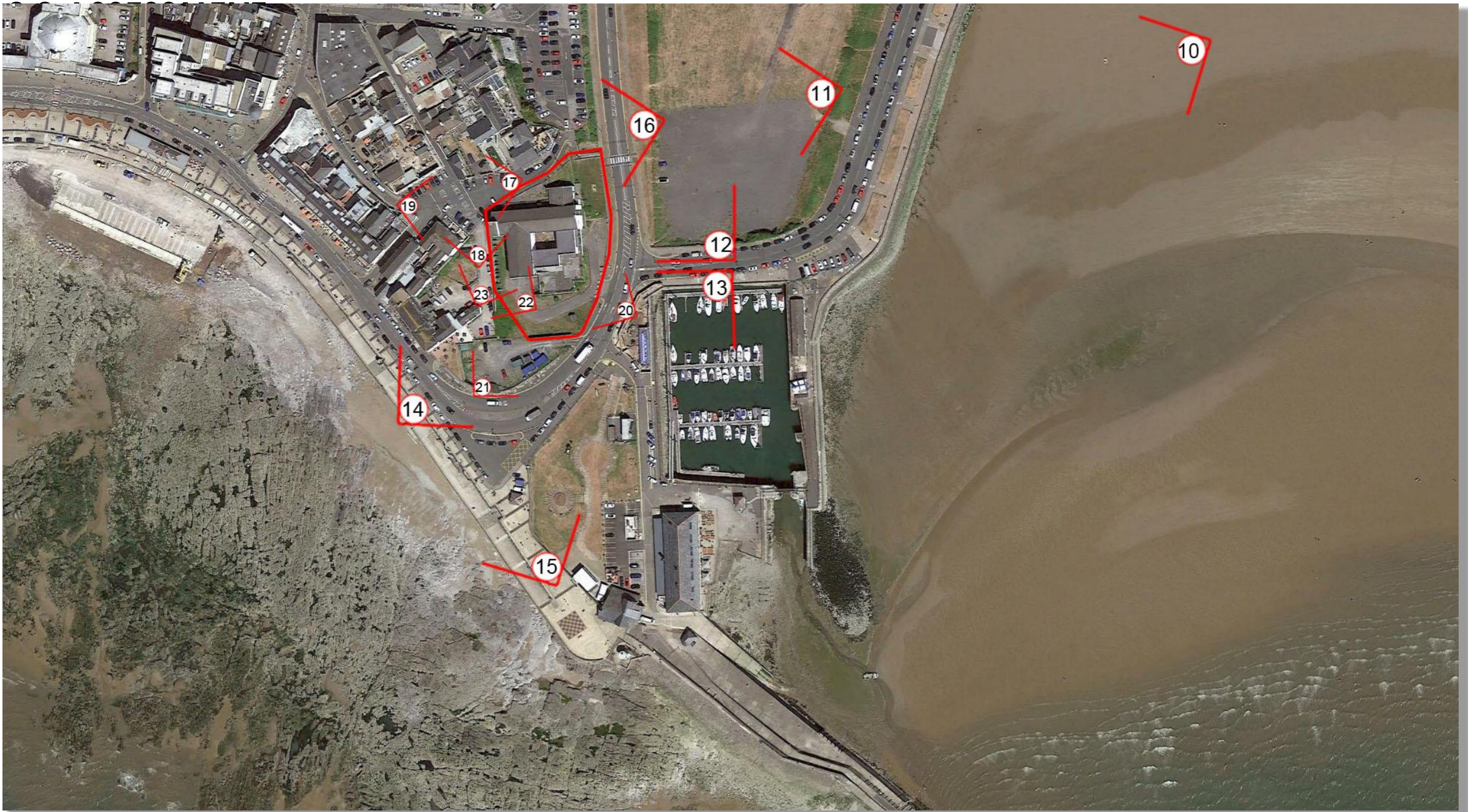
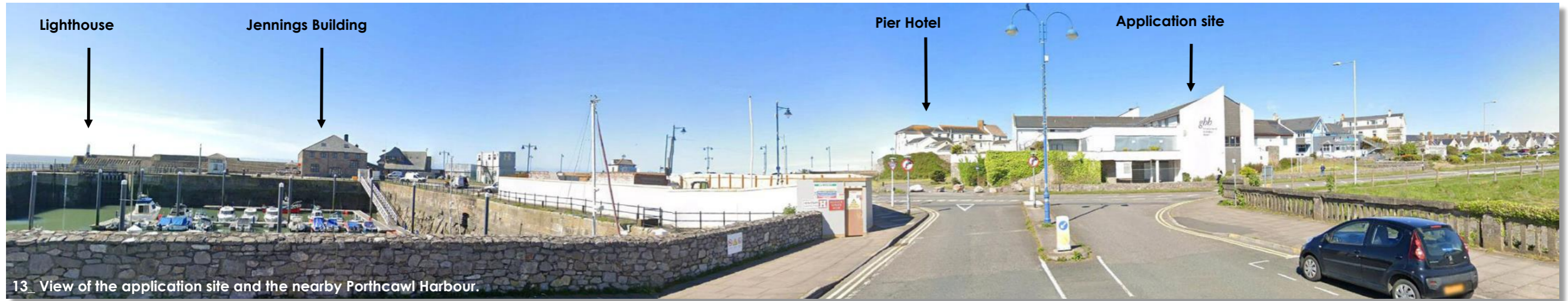


Photo key plan







16 View of the pedestrian approach to The Square as seen from the Portway



17 The Square



18 View of The Square from the lane linking The Square and the Pier Hotel.



19 View of the existing hotel as seen from The Square



20_ Western view of the hotel showing the existing vehicular access point off the Eastern Promenade



21_ Southern view of the hotel showing the vacant site called Harbour View Site.



22_ View of the retaining wall separating the site from the upper grounds



23_ Eastern view of the hotel showing the top of the retaining wall separating the hotel from the lane linking The Square and the Pier Hotel.

2.0 ASSESSMENT

2.3 SOCIAL & ECONOMICAL CONTEXT

Porthcawl lies in the County Borough of Bridgend.

The 2001 Census Data splits Porthcawl into four ward areas; Porthcawl East, Porthcawl East Central (where the application site is located), Porthcawl West and Porthcawl West Central. Combined, the four Porthcawl wards have a total population of 22,611, made up of 10,603 males and 12,008 females.

Porthcawl has a lower percentage of children and young adults than the Council area, and a significantly higher percentage of those people in the retirement age group (65+).

Porthcawl has a lower unemployment rate and a significantly higher proportion of its population possess degree level or higher qualifications than the average within the Bridgend County Borough Council area.

Although Porthcawl seems to display indicators of prosperity well above the average within the Council area, the ward within which the site area sits, "Porthcawl East Central", is far less prosperous than Porthcawl as a whole, with higher levels of unemployment, lower levels of formal education and lower levels of owner occupation.

Tourism is the key element in the local economy generating £18M each year and directly supporting in excess of 845 jobs. It is estimated that there are 38 serviced establishments in Porthcawl supplying 477 bedrooms with 892 bed-spaces, 292 of them with private bathrooms.

In the Porthcawl Development Framework it is recognised that "The regeneration of the foreshore and former dockland environments provides a unique and exciting opportunity for the community to create a vibrant new focus that will create social, economic and environmental benefits for the town itself and the wider area.

The scale of the regeneration site; its southerly outlook; its expansive views of the Bristol Channel and English coast beyond; and its relationship with the existing town create a set of local characteristics that are unique not only in the context of Welsh seaside towns but within the UK itself.

2.0 ASSESSMENT

2.4 POLICY CONTEXT AND INVOLVEMENT

Particular attention has been paid to ensuring that the development will function well and add to the overall character and quality of the area in which it is located, not just in the short term but for its whole lifetime. The scheme responds to its local context and will help to reinforce local distinctiveness and be visually attractive as a result of its architecture and appropriate landscaping.

This section sets out a summary of the relevant national and local planning policy and guidance as it relates to design and access issues.

The documents listed below are the main national, relevant policy and guidance documents which have influenced the evolution of the proposed scheme:

- National Planning Policy Framework 2018
- Design and Access Statements (CABE)
- 'By Design' Urban Design in the Planning System
- 'By Design' Better Places to Live
- Planning Policy Wales
- Technical Advice Note (TAN) 12 (design)

- Design and Character Supplementary Planning Document (April 2012)

"The general aim of TAN 12 with respect to housing design and layout is to:

- create places with the needs of people in mind, which are distinctive and respect local character;
- promote layouts and design features which encourage community safety and accessibility;
- focus on the quality of the places and living environments for pedestrians rather than the movement and parking of vehicles;
- avoid inflexible planning standards and encourage layouts with reduced road widths;
- promote energy efficiency in new housing;
- secure the most efficient use of land including appropriate densities; and,
- consider and balance potential conflicts between these criteria.

"By Design" sets out to promote and guide higher standards of urban design. The first objective of urban design is defined as character - a place with its own identity.

"By Design" outlines six requirements of development to deliver this objective, which are considered to be particularly appropriate to these proposals:

- Considering the site's land form and character when laying out new development.
- Integrating new development into its landscape setting reduces its impact on nature and reinforces local distinctiveness.
- Responding to the existing layout of buildings, streets and spaces ensures that adjacent buildings relate to one another, streets are connected and spaces complement one another.
- Responding to local building forms and local patterns of development in the detailed layout and design helps to reinforce a sense of place.
- The use of local materials, building methods and details is a major factor in enhancing local distinctiveness.

2.0 ASSESSMENT

2.4 POLICY CONTEXT AND INVOLVEMENT

- The scale, massing and height of the proposed development should be considered in relation to that of adjoining buildings, the topography, the general pattern of heights in the area, the views, and the landmarks.

In accordance with national policy the proposed design takes the opportunities available for improving the character and quality of an area and the way it functions. Particular attention has been paid to ensuring that the development will function well and add to the overall character and quality of the area in which it is located, not just in the short term but for its whole lifetime. The scheme responds to its local context and will help to reinforce local distinctiveness and be visually attractive as a result of its architecture and appropriate landscaping.

Local Planning Policy

Bridgend Local Development Plan (2013)

Planning policy within Bridgend County Borough Council is defined by the Bridgend Local Development Plan (LDP) 2013.

The most relevant Policies relating to the proposed development in the adopted Bridgend LDP are:

- Strategic Policy SP2 – Design and Sustainable Place Making
 - Strategic Policy SP3 – Strategic Transport Planning Principles
 - Policy SP5 – Conservation of the Built and Historic Environment
 - Policy SP10 - Retail and Commercial Hierarchy
 - Policy PLA1 – Settlement Hierarchy and Urban Management
 - Policy PLA3 – Regeneration and Mixed-Use Development Schemes
 - Policy PLA11 – Parking Standards
 - Policy COM3 – Residential Reuse of Building or Land
 - Policy COM5 – Affordable Housing
 - Policy COM11 - Provision of Outdoor Recreation Facilities
 - Policy REG13: Protection of Existing Tourist Accommodation
- Relevant Supplementary Planning Guidance (SPG) are:
- SPG02: Householder Development
 - SPG08: Residential Development
 - SPG17: Parking Standards

The site is shown as being within an area allocated for Regeneration and Mixed Use Development Schemes (to be assessed against LDP policy PLA3). It is also shown as being within the Porthcawl Conservation Area (SP5(2)).

The site lies in a retailing and commercial centre (Retail & Commercial Hierarchy -SP10).

Policy PLA3 promotes *the regeneration of brownfield and under-utilised sites within defined settlements that provide an appropriate mix of land uses.*

A review of the key relevant planning policy provisions for Bridgend County Borough Council has been undertaken, and is summarised below.

Strategic Policy SP2: Design and Sustainable Place Making sets out the requirements for development to *"contribute to creating high quality, attractive, sustainable places which enhance the community in which they are located, whilst having full regard to the natural, historic and built environment."* The policy describes 15 criteria on how this is to be achieved.

Strategic Policy SP3: Strategic Transport Planning Principles states that development *"should promote safe, sustainable and healthy forms of transport through good design, enhanced walking and cycling provision, and improved public transport provision."*

2.0 ASSESSMENT

2.4 POLICY CONTEXT AND INVOLVEMENT

Strategic Policy SP5: Conservation of the Built and Historic Environment recognises that *"development should conserve preserve, or enhance the built and historic environment."*

Strategic Policy SP10 Retail and Commercial Hierarchy states that *"all new development proposals within retailing and commercial centres should provide retail, community or commercial floorspace on the ground floor."*

Policy PLA1 Settlement Hierarchy and Urban Management recognise Porthcawl as a main settlement which has *"a strong employment function with an existing concentration of business and a good variety of retailing and community services that meet the needs of the settlement and the surrounding area"*

Policy PLA3 Regeneration and Mixed-Use Development Schemes states that *"the regeneration of brownfield and under-utilised sites within defined settlements that provide an appropriate mix of land use will be permitted."*

Policy PLA11 Parking Standards states that *"all development will be required to provide appropriate levels of parking"* and refer to the Council's Supplementary Planning Guidance on Parking for further details.

Policy COM3 Householder Development states *"Residential developments within settlement boundaries defined in Policy PLA1 on 'windfall' and 'small scale' sites for the conversion of existing buildings, or the re-use of vacant or under-utilised land will be permitted where no other LDP policy protects the building or land for an existing or alternative use"*. The development site is classified as a windfall site and will need to contribute to affordable housing provision as the sites meet the thresholds detailed in Policy COM5.

Policy COM5 Affordable Housing which target the affordable housing contribution for Porthcawl at 30%

Policy COM11 Provision of Outdoor Recreation Facilities states *"All new housing developments will be expected to include an appropriate level of green space for public 'amenity' purposes in the interest of good design."*

SPGs/ Development Briefs/ Design Guides

Supplementary Planning Guidance (SPG) has been prepared by Bridgend County Borough Council to expand on the policies adopted in the LDP.

SPG 02: Householder Development

SPG 08: Residential Development. The main purpose of the document is to improve the standard of design in residential development, regardless of its scale. It *"convey the design implication of TAN 12 to anyone proposing new residential development"*.

SPG 17: Parking Standards. This sets out the standards that are sought by the Authority for car parking provision in various areas and for a range of different types of development. It recognises that in certain circumstances, a reduction in car parking provision associated with new development is appropriate.

This DAS provides an assessment of how the proposed development successfully achieves high sustainable development credentials in accordance with the foregoing national and local planning policy requirements.

2.0 ASSESSMENT

2.4 POLICY CONTEXT AND INVOLVEMENT

Justification for planning policy departure.

Policy SP10 – Retail and Commercial Hierarchy states that: 'all new development developments within retailing and commercial centres should provide retail, community or commercial floorspace on the ground floor'.

Whilst the proposed site lies within the defined Porthcawl retailing and commercial centre it is felt that the use classes are not sustainable in this location.

No retailing, commercial or community uses are currently present on site so there is no loss impact from the proposal.

The site is situated on the southern most limits of the area classified as Porthcawl retail and commercial centre with most retail premises being located further north.

There are very few retail or commercial building around the proposed site reducing retailing footfall and the prospects of a successful commercially viable venture.

The concept of placing retail, commercial or community uses on site were assessed during the initial stages of the project and found to be unsustainable.

Several site constraints including levels, limited accessible site frontage, constrained vehicular and pedestrian access points, footfall and limit opportunity to safely service the proposals all led to the possibility of achieving a successful retail or commercial scheme being unviable.

In reality the inclusion of retail, commercial or community uses being included at ground floor within the proposal would result in empty unused space that is unattractive to the commercial market.

Policy REG13 – Protection of Existing Tourist Accommodation.

The site is currently occupied by the former Glamorgan Holiday Hotel which has been vacant since October 2016 when it closed its doors for the last time. The Hotel served holiday makers for around 42 years.

The hotel was opened as the Glamorgan Holiday Hotel in 1974 by three local county councils to provide respite holidays and short breaks for the elderly and frail in South Wales.

The hotel offered flexible arrival and departure days with care being on hand 24 hours a day to provide levels of support to suit everyone's needs.

When local government was re-organised in 1996, the councils set up a charitable trust to run the hotel, initially with block contracts to pay for people in need of a holiday with care and support.

Gradually the council contracts were withdrawn and the hotel was forced to become financially more self-supporting.

2.0 ASSESSMENT

2.4 POLICY CONTEXT AND INVOLVEMENT

In 2012 Cartefi Cymru became involved in the hotels operation before it being full taken over by Cartefi Cymru in 2015. In early 2015 the hotel underwent a £1,000,000 overhaul, refurbishing the accommodation and providing new facilities.

It was hoped that the investment would draw custom to the hotel; however this never materialised and following a very quiet summer in 2016 it was forced to shut.

Investigations into the closure found that the requirement for specialist support holiday accommodation had declined with the increase in other hotels becoming more accessible and inclusive.

The existing accommodation has fallen into a state of disrepair with a substantial amount of investment required to re-instate it.

Given the reasons for the hotels closure and the investment required to re-instate it, hotel use on this site is not considered to be commercially appropriate or sustainable.

The site was marketed in 2016 / 2017 and was purchased by Xcape Limited. Since the 2017 purchase the building has been vacant with little to no appetite to re-instate the former specialist hotel use.

2.0 ASSESSMENT

2.4 POLICY CONTEXT AND INVOLVEMENT

INVOLVEMENT

Xcape Limited is committed to consulting with the local community regarding the proposed redevelopment of the application site. The consultation process is currently being undertaken by BECG on behalf of Xcape Limited.

As part of its commitment to consultation with local communities, Xcape will be progressing the planning details of the scheme through Bridgend County Borough Council, following the period of statutory and non-statutory pre-application community consultations.

A statutory 28 days consultation period will precede the planning submission.

Community stakeholders will be notified of the triggering of the statutory 28-day consultation period by newsletter and covering letter, delivered by post and email (where available).

In parallel with the initial engagement with community and specialist stakeholders regarding the scheme, Xcape will undertake a phase of outreach and engagement focused on owners and/or occupiers of land and property adjoining the proposal site, as well as consultation with neighbouring residents and businesses in the area surrounding the site.

Neighbouring owners, occupiers, and neighbours will be notified of the triggering of the statutory 28-day consultation period by newsletter and covering letter, delivered by post.

The above letters and newsletters (which will be drafted in English and Welsh) will include all of the information on the Site Notice; state that owners, occupiers, and neighbours have at least 28 days to respond in response to the plans, providing them with means to do so; and direct them to a consultation website where all planning documents will be available to view.

To ensure that local residents and stakeholders can contact the project team, a Freephone information line number will be in operation for the duration of the project. The number – **0800 298 7040** – will be available between the hours of 9:00am and 5:30pm, Monday to Friday with a message facility for out-of-office hours.

A dedicated consultation website will be created in order for local residents and stakeholders to access information about Xcape's proposals for the site. This website will be updated throughout the consultation and will also allow people to provide comments about the proposals. Copies of all relevant draft planning application documents will be available to view and download once the 28-day statutory consultation period has been triggered.

A pre application meeting was held with the Planning Officer at Bridgend Council on the 4 July 2019 to discuss the proposal.

3.0

EVALUATION

- 3.1 CONSTRAINTS & OPPORTUNITIES
- 3.2 ASSESSMENT
- 3.3 PRINCIPLES TO BE ACHIEVED

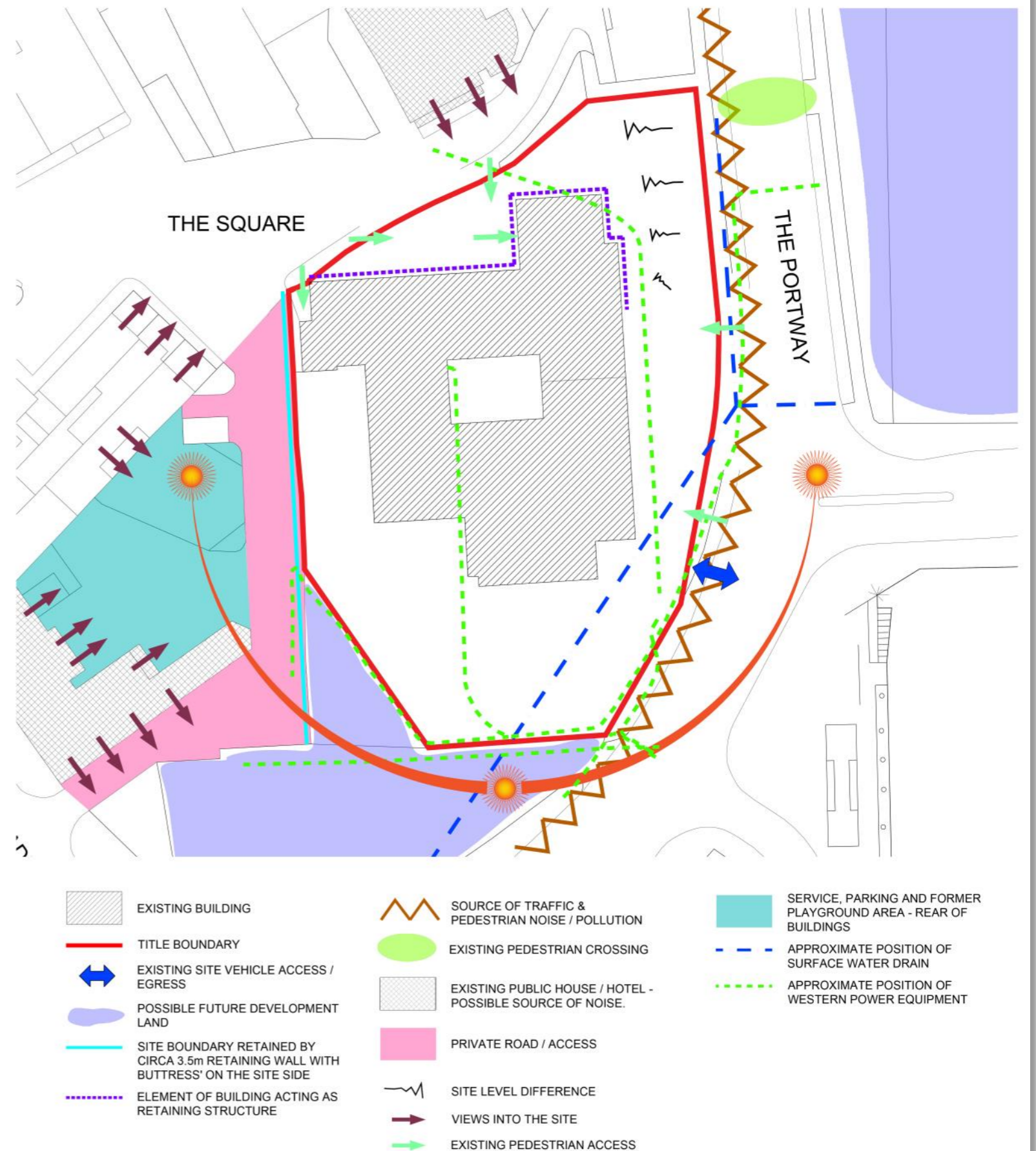
3.0 EVALUATION

3.1 SITE CONSTRAINTS AND OPPORTUNITIES

CONSTRAINTS

Listed below are some of the constraints that need to be considered when assessing the suitability of developing this area of land for residential purposes:

- The site is located within the Porthcawl conservation area and adjacent to the western development area of the Porthcawl Waterfront Regeneration Area;
- The development has two opposing frontages onto public spaces (one to the east facing the Portway and one to the west facing The Square) which will need to be addressed;
- Steep differences in level along the western and northern boundaries making connectivity with The Square challenging;
- Existing vehicular site access in close proximity with junction between The Portway and the Eastern promenade;
- Noise generated by the nearby road network and tourist attractions;
- Coastal environment (challenging for building materials and planting);
- Varied scale, mass and height of neighbouring properties;
- Limited space available for car parking;
- Demolition of existing buildings required (pre 2000, asbestos).
- Existing services crossing the site;
- Limited accessible site frontages / access points;
- Possible future development sites surround the proposed site.



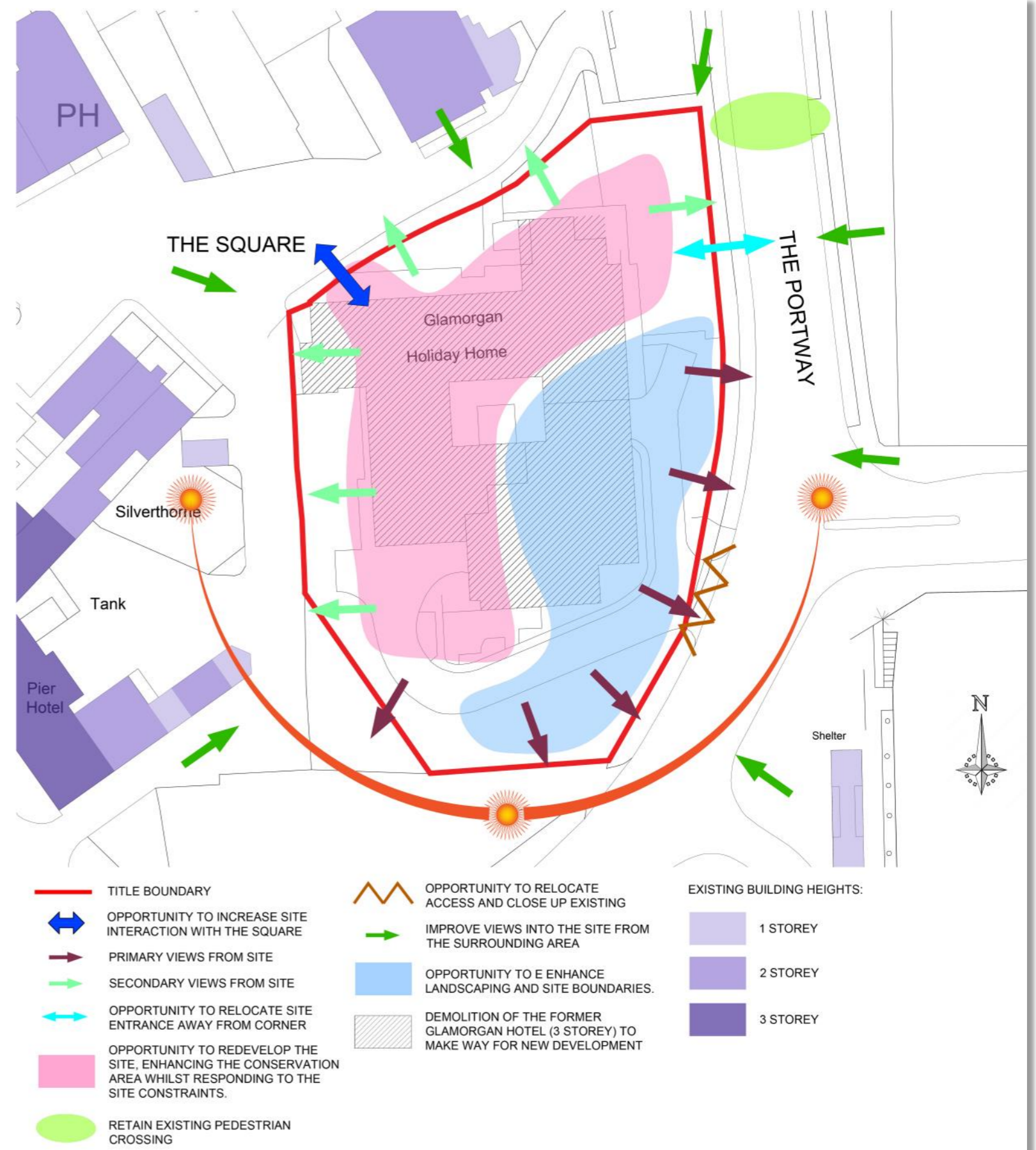
3.0 EVALUATION

3.1 SITE CONSTRAINTS AND OPPORTUNITIES

OPPORTUNITIES

Listed below are some of the opportunities that have been considered when assessing the suitability of developing this area of land for residential purposes:

- To regenerate a brownfield site;
- To design a building that makes best use of the varied levels across the site;
- To maximise the uninterrupted distant views the site affords over Sandy Bay and the Bristol Channel;
- To provide much needed housing;
- To provide a new modern building;
- To manage and improve landscaped amenities that will benefit the future users and the existing neighbouring properties;
- To improve access;
- To provide appropriate on site car parking provision;
- To enhance a key site on the main approach to the Esplanade.



3.0 EVALUATION

3.3 PRINCIPLE TO BE ACHIEVED

The diagrams on the previous pages illustrate the opportunities and constraints that are relevant to the application site and inform the design principles. The design should respect and respond to the character and appearance of the surrounding area and the position and massing of the adjacent buildings. The design should also comply with, and satisfy, all the general functional requirements implicit with the provision of affordable accommodation and open market housing.

Design principles:

- design buildings of their time that positively responds to the quality of the context;
- create scale and proportion relevant to people and the surrounding buildings and spaces;
- respond to the proximity and scale of adjacent buildings;
- respond to key focal points, views and vistas;
- clearly distinguish between private and public space;
- use quality materials and detailing.

4.0

THE DESIGN

- 4.1 USE AND AMOUNT
- 4.2 LAYOUT, SCALE AND APPEARANCE
- 4.3 MATERIALS
- 4.4 LANDSCAPE DESIGN
- 4.5 ACCESSIBILITY AND SUSTAINABILITY

4.0 THE DESIGN

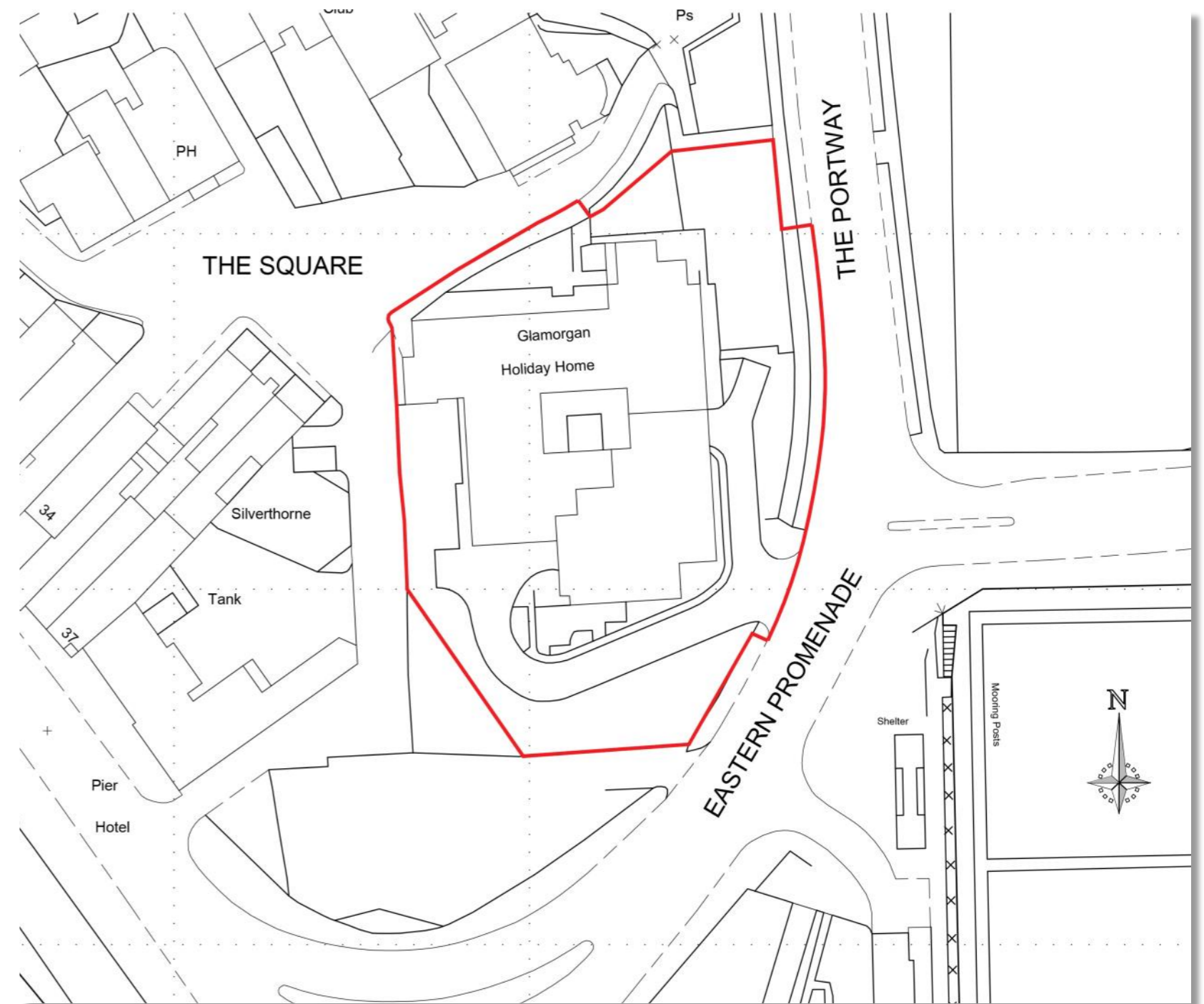
4.1 USE AND AMOUNT

This application is seeking full planning permission for an apartment complex with associated facilities and car parking.

The proposed application site measures circa 3705m²/0.37 hectares/0.92 acres.

The apartment complex scheme (gross external area of 5514m²) is composed of:

- 54 apartments (28No 1 bed, 21No 2bed, 5No 3 bed) 16 of which are allocated to affordable housing;
- refuse and recycling store;
- bicycle store;
- 64 total car parking spaces;
- landscaped communal gardens and other associated facilities.



Application site boundary diagram (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

LAYOUT (MASTERPLAN)

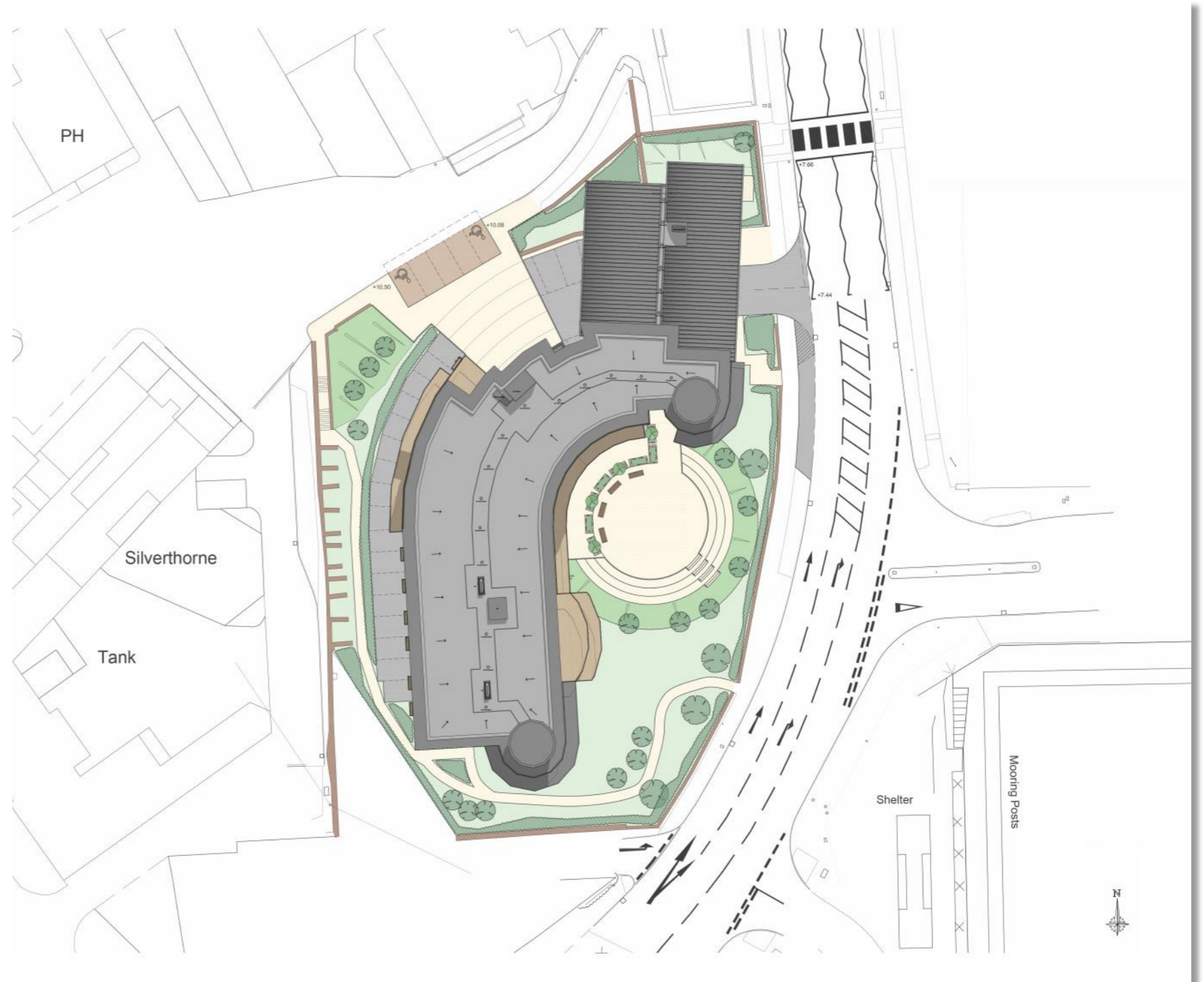
The site constraints and opportunities diagrams, discussed earlier in this report, led to a concept design which informed the final layout of the proposal

The key points of the proposal are:

- creating an interesting and vibrant frontage that responds well to the surroundings ;
- address the limitation inherent to the existing access points and the difference in levels;
- enhancing existing landscaping and boundaries;
- maximise boundary planting;
- providing adequate private car parking on site
- embracing the opportunity of distant views over the Porthcawl Harbour, Sandy Bay and the Bristol Channel;

Taking these factors into account, the layout has been developed to establish the optimum use for the site.

Circular 01/2006 defines layout as the way in which buildings, roofs and open spaces (both public and private) are provided, placed and oriented in relation to each other and buildings and spaces surrounding the development.



Proposed site layout at roof level (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

The development layout as proposed is a logical and straight forward response to the constraints of the site and its relationship with neighbouring properties, the local townscape context and also to the Client's brief.

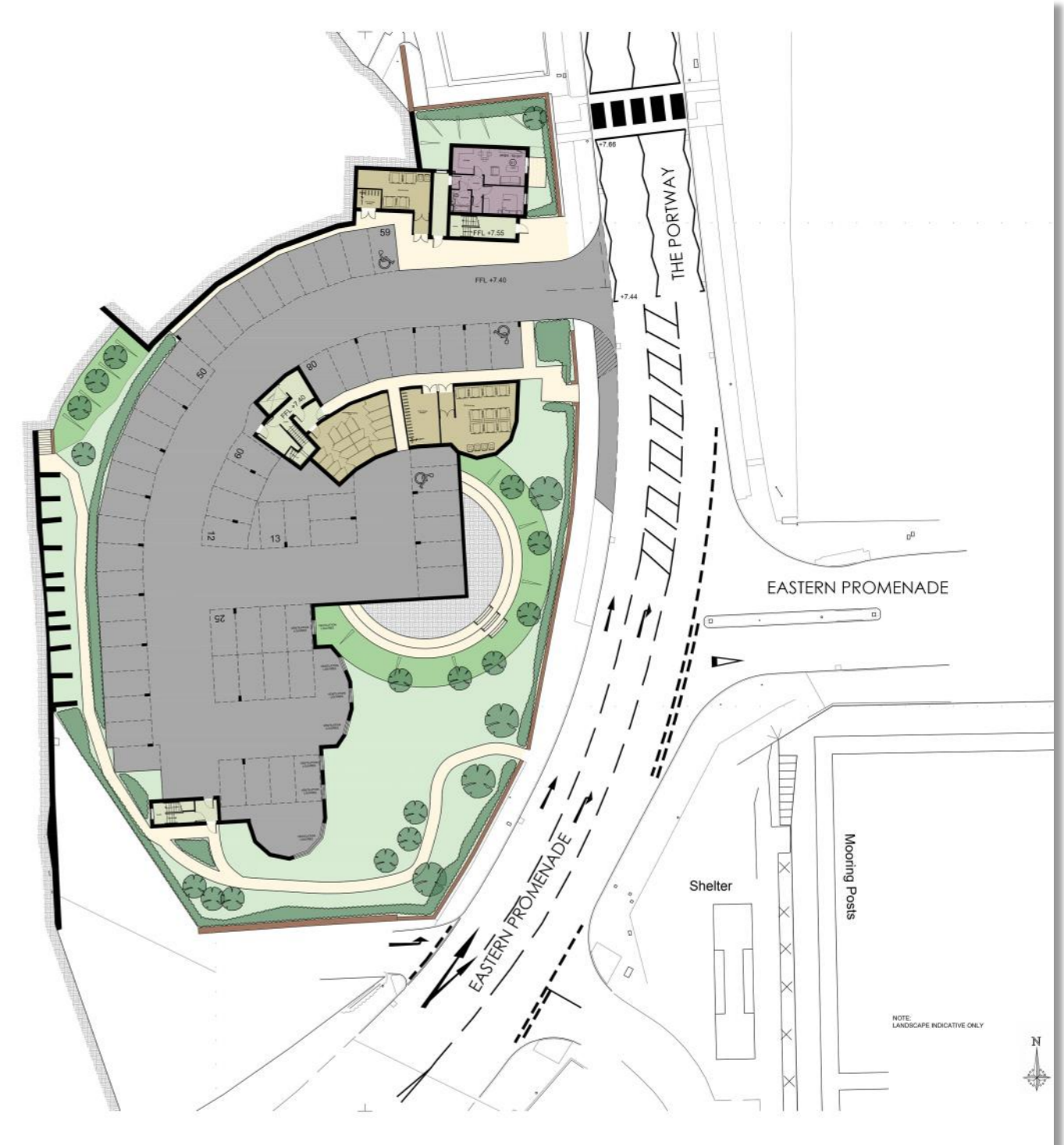
In addition, the layout has evolved to achieve the following:

- provide an appropriate well designed response for the redevelopment;
- Fulfil the need of a double frontage giving equal design responses to The Portway and to The Square;
- provide amenity space for residents and the wider public;
- improve legibility and facilitate ease of movement.

The proposed development entails the demolition of the existing buildings and the construction of a new apartment complex .

The proposed building extends over 5 floors and is separated into two blocks of accommodations, one for the open market apartments (5 Floors) and one for the affordable homes (4 floors).

The lower ground level is located at the same level of The Portway, circa 3 meters below the level of The Square. Located off The Portway is the pedestrian access to the affordable apartments, the main vehicular access, car parking, refuse and cycling storage.



Proposed site layout at lower ground floor level. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

The ground floor level is located at level with The Square. Located off The Square is the main pedestrian access to the open market apartments. The remaining upper levels are all dedicated to accommodation.

The layout had to address the challenges related to access for pedestrian, cars, refuse collection vehicles, fire engine and deliveries.

The existing vehicle access point located off the Eastern Promenade and the main frontage of the site is proposed to be relocated. The proposed access is moved further north away from the junction of The Portway and the Eastern Promenade creating a safer access and egress point for the development.

It has been carefully considered to ensure that the correct visibility splays are achieved for vehicles arriving and leaving. It is 6 m wide, ensuring the easy flow of traffic and minimising disruption to the present and future traffic flow along The Portway.

Alongside the vehicular access is the proposed pedestrian access point for the affordable apartment block.

A layby lane has been located immediately south of the vehicular access to allow for refuse collection and emergency vehicles who will only be present for short period of time.



Proposed site layout at ground floor level. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

A secondary frontage is proposed to the north west side of the site contributing to the townscape of The Square. Here the main building is detached from the retaining walls holding up the higher grounds and linked with The Square by a pedestrian elevated link which grant pedestrian access to the open market part of the development. On the upper floors accommodation is located on both side of the building with a central corridor running north to south. The apartments are a mix of one, two and three bedroom units.

Landscaping and amenities areas are extremely important for this type of accommodation. A dedicated landscape scheme is included within the submission and described in detail in section 4.4 of this document.



Proposed eastern elevation along The Portway/ Eastern Promenade. (NTS)



Proposed north western elevation fronting The Square and the pedestrian link with The Portway. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

Height, Scale and Massing

The scale, height and massing of the building has been influenced by the location and setting of the site, constraints and opportunities posed by the immediate and wider context, sound urban design principles and the local urban design objectives. Scale is the height, width and length of a building in relationship to its surroundings.

The main challenge in designing an appropriate proposal for this site has been to find a balance between the space available for the accommodation, an appropriate architectural response in scale, height and mass, and the brief requirements.

A large influencing factor has been the dramatic level change between the main development site and its northern and western boundaries. This element has been a major consideration in integrating the proposal effectively into the wider setting.

The proposal has been conceived as an “S” shaped block of accommodation set back as much as possible from all existing boundaries and the main road.

The shape of the building allows for a generous landscaped amenities space in proximity to the junction between The Portway and the Eastern Promenade, providing residents with a south facing open space to enjoy that is not overshadowed. The space further benefit from views across the Bristol channel and beyond.

The development includes a range of one, two and three bedroom apartments over 4 floors (affordable block) and 5 floors (open market).

This undoubtedly creates a building with a larger mass than the existing hotel.



Proposed western elevation fronting The Square and the pedestrian link with the Esplanade. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

The following points have been utilised to keep the mass of the proposal as appropriate as possible:

- the five floor element of the proposal has been set away from the main road frontage and separated from it by a generous landscape buffer;
- the top level has been recessed to minimise its perception when seen from street level;
- to the north the height of the building has been reduced to 4 floors to minimise its impact on the neighbouring properties;
- all parapet heights have been minimized to reduce the overall height of the building.

As with all developments there are two issues that need to be considered when assessing scale. The first is the primary scale, which is established by the width and height of the proposed building or its primary elements.

The other is the secondary scale, which is established by the size and spacing of individual elements that relate to the human interaction with the building, such as doors and windows.

Secondary scale is equally as important as primary scale, as it affects the way in which the actual size and primary scale of a building is perceived. For example, a building with a small secondary scale is likely to be perceived as being smaller than a similarly sized building with a large secondary scale.



View of the southern elevation. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

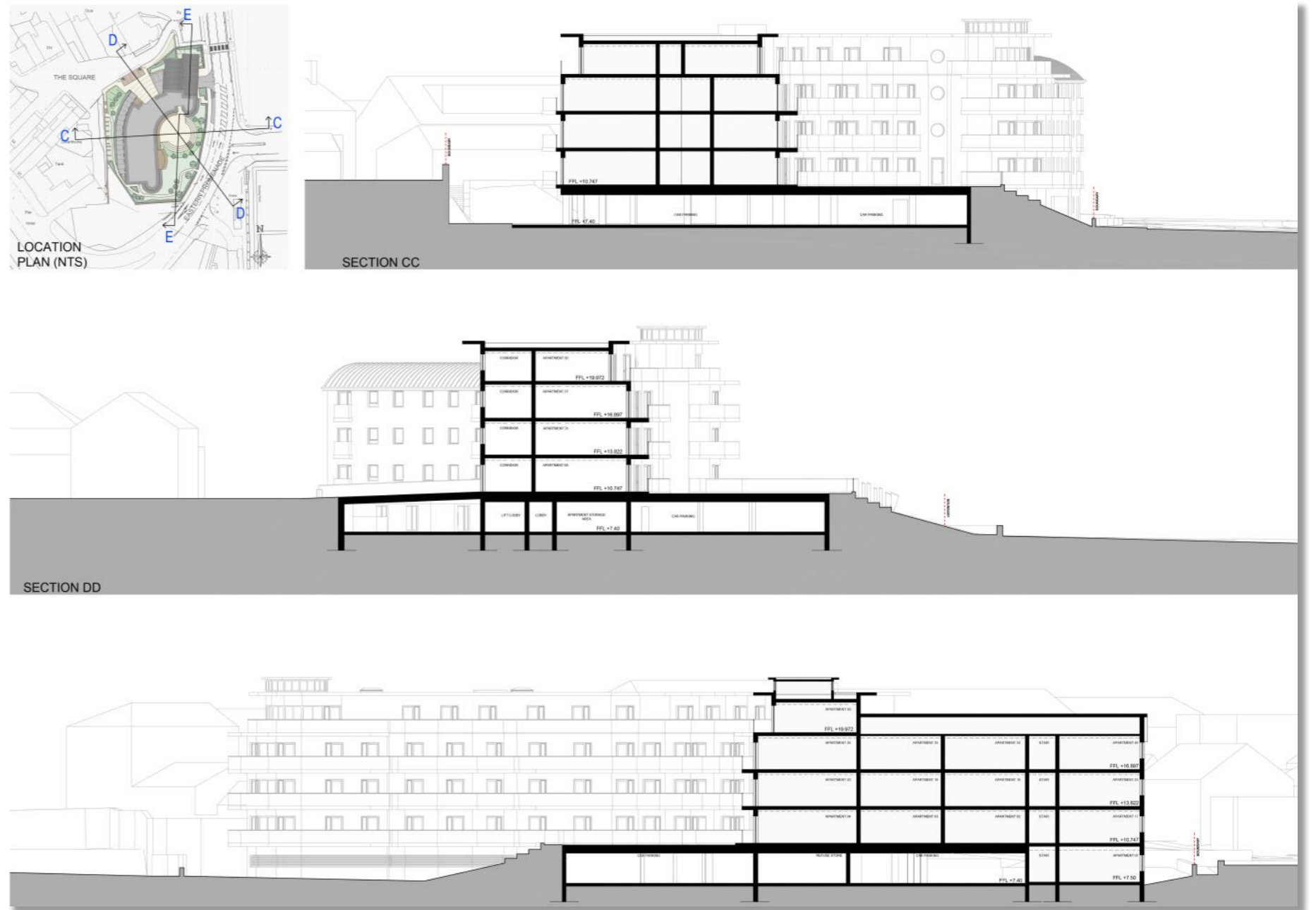
The mass of the proposal is broken down into domestic size elements that relate well to the surrounding residential properties. Assorted parapets heights, balconies and projecting elements, all add to the varied articulation that gives the elevation characterisation synonymous with a domestic scale architecture.

The secondary scale elements can clearly be read, with fenestration that punctuates the facades and reinforces the human scale. The secondary scale is entirely domestic, giving reference to the function and status of the elements themselves, which is appropriate to the context in which the site is set.

The level difference has been embraced to minimising the massing of the building whilst assisting to create an undercroft area to reduce the dominance of vehicle parking.

The proposed landscape scheme along the eastern frontage of the site will play an additional part in securing a proportionated integration of the buildings mass in its surroundings.

Taking into consideration the location of the proposed building and the distance from its boundaries, its proposed height can be perceived as an appropriate scale, in keeping with nearby properties and the local context as a whole.



Cross sections showing the relation between the building and the site levels.

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

The overall height of the proposal is considered appropriate to this setting and as such enhances the area with a considered, contextual response.

Government guidance in "By Design" suggests that whilst building heights might need to respond to a general pattern of heights in the area and to matters of good neighbourliness, the need to provide high residential densities to create vitality and make the most of public transport are important considerations too.

The proposal achieves this by delivering an appropriate quantity of accommodation and taking into account the constraints of the site.

The design of the proposed apartment building is a careful balance between the achievement of sustainable and environmental criteria and the setting of the buildings within the existing landscaping. The mass and scale of the development was also informed by the appropriate capacity of the site and local area.



Perspective view of the eastern frontage of the development along The Portway. (NTS)



Perspective view of the northern eastern corner of the development. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

APPEARANCE

Appearance is the aspect of a place or building that determines the visual impression it makes and includes the external built form of the development, its architecture, materials, decoration, lighting, colour and texture. As set out in the design objectives, the proposal aims to create a building that positively responds to the quality of the context and creates a frontage that respects the scale, massing and appearance of the surrounding environment.

This has been addressed through the articulation of the façade and the choice of materials.

The inspiration for the design of the proposed building comes from many sources, all local to Porthcawl.

The linear composition highlighted by the generous balcony lines recall the nearby coastline, a combination of rolling hill tops above horizontal limestone strata punctuated by beaches and safe ports.

In its hay day post-war years of the 1930's it became a bustling holiday resort a popularity it is working hard to maintain. The design references this age in its building form from buildings such as the Sea Bank Hotel and Continental Café.

With a concentration on smooth lines, curved forms and an openness of fenestration, giving the design a welcoming friendly



Perspective view of the western frontage showing the relation of the development with The Square (NTS)



Perspective view of the site as seen from the south. (NTS)

4.0 THE DESIGN

4.2 LAYOUT, SCALE AND APPEARANCE

demeaner. The horizontality reflected in the cliffs nearby, the ships which passed into and out of the harbour and seen all along the board walk are also reflected in the building, giving a familiar rhythm. This openness gives the occupants the maximum advantage of a view befitting of its location, and the opportunity to see as well as be seen in this boardwalk town.

The proposed design alternates the use of different materials to highlight the varied elements of the building.

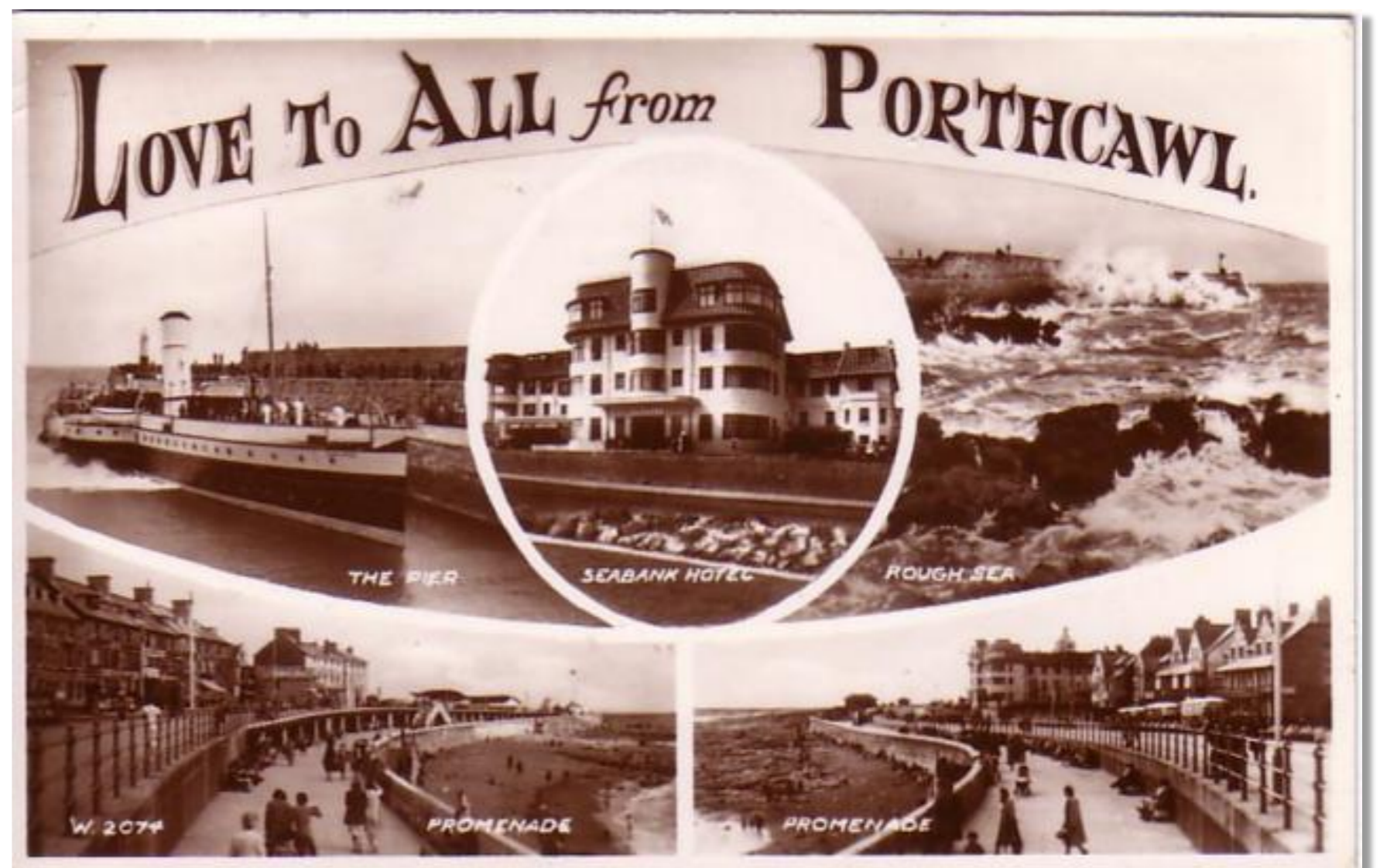
Elements have been 'pushed' and 'pulled' to create depth and shadows, and to give visual interest, as a result a varied architectural form is created which breaks up the overall massing and portrays a domestic streetscape.

Through the use of different forms and contrasting materials the elevation facing The Portway has been modelled to create a linear arrangement, giving to the development a balanced and easily legible finish creating a new focal point which will highlight and frame this part of Porthcawl.

The proposed materials and their arrangement have been designed to accentuate a light and shade approach reducing the perception of its mass.

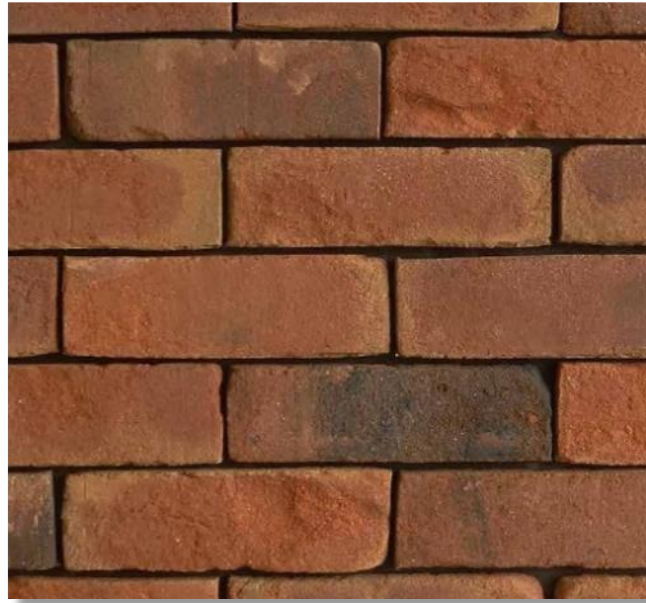
The windows are variously sized and arranged and the fenestration pattern throughout the building has been carefully considered to establish an appropriate solid-to-void ratio.

Finally, the building has been designed not to prejudice any development that may come forward on the abutting site in BCBC ownership.



4.0 THE DESIGN

4.3 MATERIALS



1. Red multi stock brick



2. Acrylic Based Render (OFF WHITE)



3. Cladding panel (GREY)



4. Powder-coated metal balconies (DARK GREY) with glass balustrades



6. Standing seams metal vaulted roof (DARK GREY)



7. UPVC window frames (DARK GREY)



8. Skyline fascias (DARK GREY)

4.0 THE DESIGN

4.4 LANDSCAPE DESIGN

Landscaping-strategy

The landscape strategy seeks to:

- Integrate the new development with its surroundings, taking careful account of the site levels and the impact of the development on neighbouring areas.
- To create an external environment that enhances the townscape in this locality, while also providing an appearance that can be enjoyed by residents and visitors.
- Include good boundaries and hedge buffering to existing properties that surround the site to minimise any impacts upon them.
- Provide landscape areas and seating that respond to the particular aspect and micro-climates that the buildings will create, so providing robust and healthy landscapes that will be easy to maintain and attractive.
- Provide a communal garden that responds to the suns movement providing sunny seating areas and opportunities for relaxing and meeting in small groups.

- Allow for easy movement of people and deliveries from car park areas and for level and safe emergency egress where necessary.
- Tree Planting and soft landscaping

Trees and Planting

Tree planting is designed to respond to the particular functions that the landscaping needs to achieve around the site. The soft landscaping around the site and comprises a mixture of shrubs, grasses and herbaceous plants that will provide a wide range of seasonal interest.

The site is well defined by existing stone boundary walls which are all to be retained, and upgraded where required.

Proposed planting will be designed using robust species that have good potential to thrive on this site, fulfilling their intended function with relatively low levels of maintenance.

Boundary planting will comprises a high proportion of evergreen species to provide all year round screening.

Around the buildings and in amenity areas, more colourful planting will be provided, including a variety of

herbaceous material in order to provide visual diversity, colour and interest at different seasons.

All planting will be selected to be suitable to the prevailing soil conditions of the site and from species tolerant of a degree of maritime exposure / salt spray.

The building position to the rear of the site has been dictated by the need to maximise views, exposure to light and sun and to increase the space available for landscaping and amenities spaces.

One of these spaces has been created towards the centre of the building along the eastern elevation. Here a raised platform creates a terrace that can be enjoyed by the residents while at the same time covering the parking spaces below.

Should the scheme be consented planning permission a full and detailed landscape scheme will be submitted for approval.

4.0 THE DESIGN

4.4 LANDSCAPE DESIGN



Proposed landscape layout. (NTS)

4.0 THE DESIGN

4.5 ACCESSIBILITY AND SUSTAINABILITY

Access to the development has been carefully considered to provide inclusion for all and in consideration of:

- Supplementary planning guidance 'Inclusive Design and Access for All'
- Approved documents (incl. building regulations Part M)
- British standards where applicable

Please refer to the proposed accessibility plans within this section.

MOVEMENT AROUND THE BUILDING

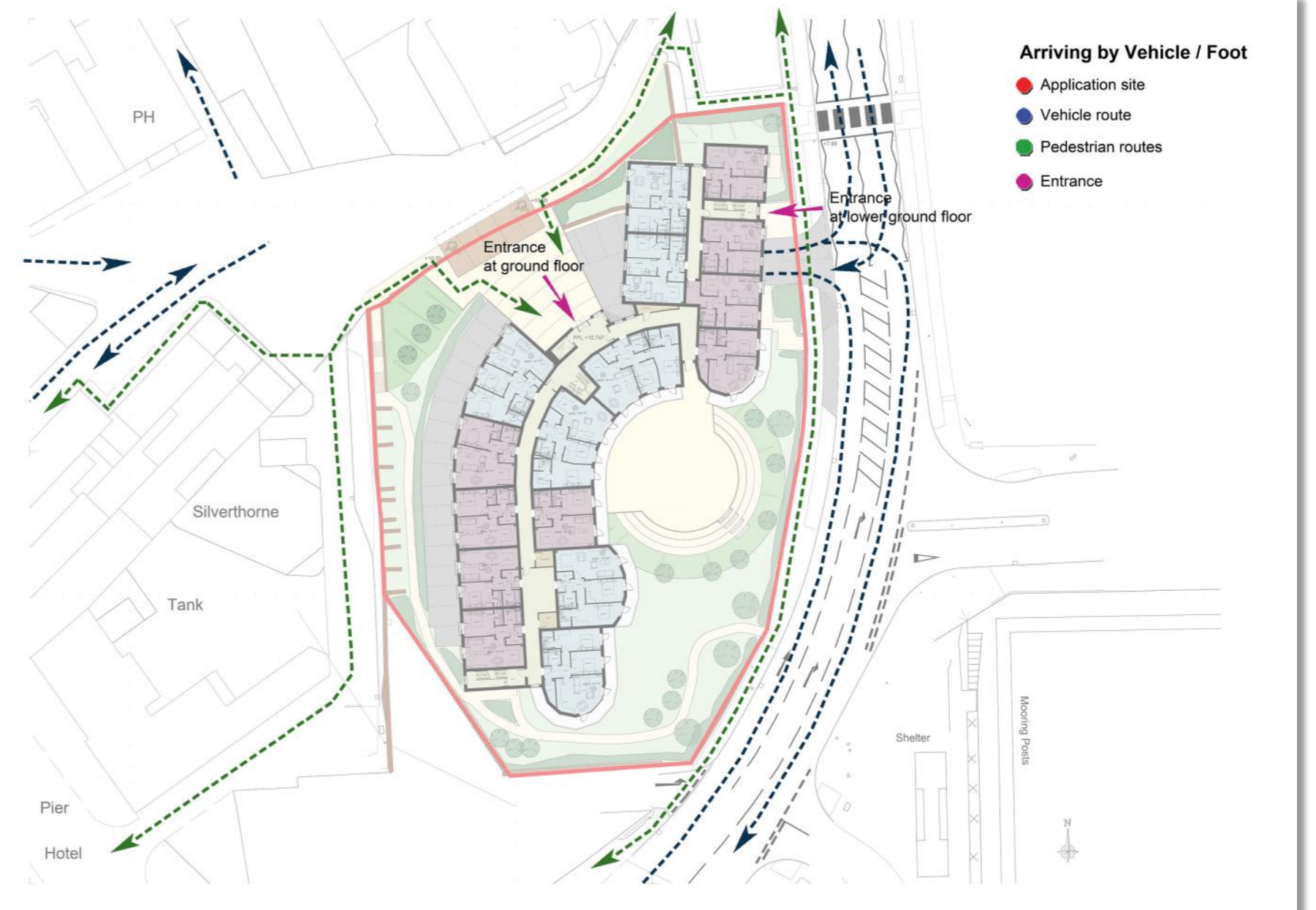
The proposed site is ideally located for its proximity to the town centre providing easy access for groceries and other amenities. There are two main pedestrian access points to the site and one vehicular access point.

The proposal is designed to suit the needs of ambulant disabled people and all of the access points are compliant with Part M of the Building Regulations.

MOVEMENTS WITHIN THE BUILDING

Level floors will be provided throughout the development with vertical connections provided via staircases or lift.

Staircases are designed to suit the needs of ambulant disabled people in accordance with the Building Regulations.



Accessibility plan: Vehicular and Pedestrian

4.0 THE DESIGN

4.5 ACCESSIBILITY AND SUSTAINABILITY

Corridors are of a suitable width to allow wheelchair users to pass. The apartments themselves have door widths sufficient to allow access by wheelchairs.

REFUSE AND RECYCLING

The facilities for refuse and recycling have been provided within the development. There are two dedicated refuse stores close to the site entrance which contain bins for recycling and refuse.

ACCESSIBILITY OF EMERGENCY SERVICES AND REFUSE TRUCKS

Access for emergency services and refuse trucks will be via the dedicated layby located alongside the main vehicular entrance off The Portway. Emergency vehicle will also be able to access the development from The Square.

FIRE AND FLOOD EVACUATION STRATEGY

Fire exit signs will be specified and placed in the appropriate locations. A comprehensive fire exit strategy will also be drawn up by the management to inform residents of the best exit routes in the event of a fire or flood.



Accessibility plan: Refuse and Delivery vehicles

4.0 THE DESIGN

4.5 ACCESSIBILITY AND SUSTAINABILITY

SUSTAINABILITY STRATEGY FOR THE DEVELOPMENT

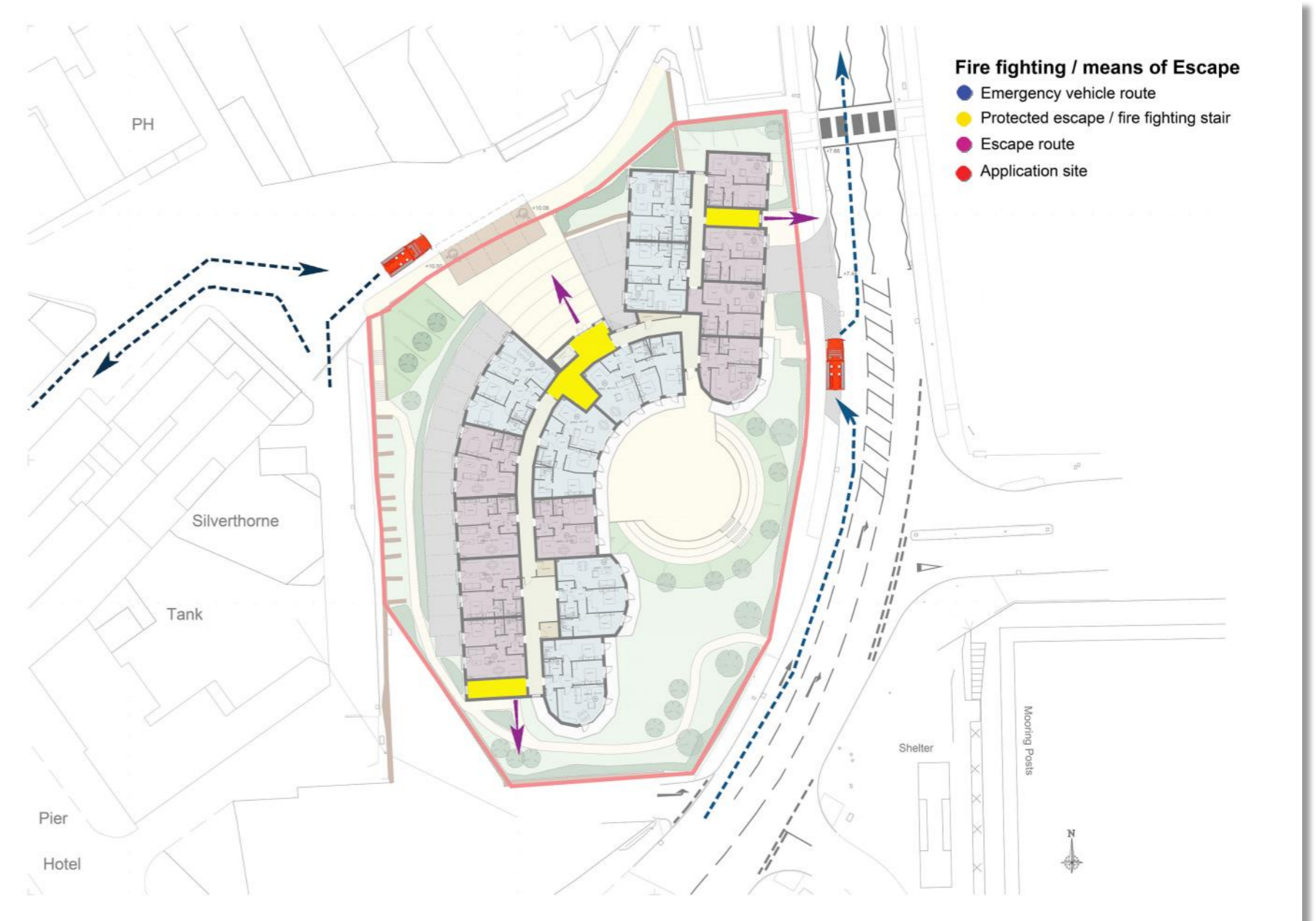
IMPACT ON THE ENVIRONMENT

Building construction and the use of buildings, especially their space heating, is one of the main contributors to CO₂ emissions in the UK. Energy conservation, introduction of sustainable transport strategies and incorporation of energy efficient strategies can be one way to play a part in reaching CO₂ emission reductions, while reducing other adverse impacts on the environment.

PUBLIC REALM

The proposal has been designed to respect and enhance the setting and acknowledging its location. The scale, height, mass and disposition is appropriate to the wider locality relevant to the context in which the site is set. At the same time, the proposal has been positioned to minimise the impact on any neighbouring properties. The layout of the pavements and spaces achieve a balance between good access into the development and providing interesting and useable spaces considering the testing terrain.

SITE LOCATION & AMENITIES



Accessibility plan: Fire fighting & means of Escape

4.0 THE DESIGN

4.5 ACCESSIBILITY AND SUSTAINABILITY

The site is well positioned for easy access to local services and amenities within the area. This proximity helps to reduce dependence on the private car, thereby reducing pollution from NOx gases and greenhouse gas emissions. The proximity will also help to encourage social inclusion and enable participation by future residents of the proposal in Porthcawl and the wider community.

TRANSPORTATION

The site benefits from easy access to public transport facilities with bus stops located within short walking distance. The proximity of the development to local amenities has the important benefit of contributing to the reduction of car dependency. A 'Green Transport Plan' will be prepared for future residents in order to inform them of local transport services and home delivery services. Advice will also be included on regional and national transport links. The combination of all these factors effectively reduces the potential for adding to air pollution and increasing vehicular fuel consumption.

For further details on transport please refer to the transport statement prepared by Corun Associates Ltd and submitted as part of this application.

DENSITY DEVELOPMENT AND DIVERSITY

DESIGN AND ACCESS STATEMENT | Apartment complex, The Square, Porthcawl, Wales.

The incorporation of a housing development within the wider residential context presents an opportunity to improve the density of development - a sustainable strategy.

SOCIO-ECONOMIC IMPACT

The location and the nature of the proposed development will help to improve Porthcawl social sustainability and its economy.

SUSTAINABLE STRATEGIES IN DESIGN

SITE ECOLOGY, BIO-DIVERSITY AND LANDSCAPING

The proposed landscape strategy plan provides a sensitively designed scheme enhancing the surroundings and creating an appropriate response to the character of the area.

LAYOUT, ORIENTATION, FORM AND MICRO-CLIMATIC FACTORS

Glazing on the south facade provides some solar gain. However, the east and west facade glazing provides a greater area to benefit from solar gain. The layout ensures that most apartments will benefit from direct sunlight at some point during the day. The form of the building has largely been dictated by the shape and topography of the site, functional requirements, site analysis, contextual analysis and historical context.

ENERGY EFFICIENCY

Each separate habitable room within the apartments will be designed with independent thermal controls as standard. Communal lighting will be linked to daylight and/or movement sensors. All the spaces within each flat will be lit by low energy light fittings, including all the external lighting. Good thermal performance will be further achieved by the use of Low-e solar control double-glazing

4.0 THE DESIGN

4.5 ACCESSIBILITY AND SUSTAINABILITY

with enhanced thermal insulation. An energy saving advice leaflet will also be passed on to the homeowners.

Thermal insulation and ventilation will be designed into the building envelope using a specification to achieve a good SAP rating in compliance with Part L of the Buildings Regulations. Combined with a good standard of ventilation and air tightness, this will help to conserve the fuel used to power the building.

COMPACT FORM AND EMBODIED ENERGY

The general use of this goes towards contributing to reduce the embodied energy, the non-renewable energy consumed in the acquisition of raw materials, their processing, manufacture, transportation to site and the construction process. The proposed configuration of apartments linked, in most part, by a central corridor also reduces the surface area to floor ratio with a corresponding reduction in the space heating energy usage. Furthermore there is some reduction in the effective area over which the building would lose this heat through its fabric. As a result of this building form, maintenance costs are also reduced particularly if there is a comparison with an equivalent number of bed spaces provided in conventional housing. In conclusion the compact form will increase the whole life

value and reduce the life cycle energy use of the proposed scheme.

HCFC (HYDRO-CHLORO-FLUORO CARBONS) EMISSIONS

HCFC's (ozone depleting substances) within the building will be reduced and where possible will not be included in the specification.

WATER ECONOMY

Dual flush 4-6 litre WCs are proposed to be specified in each apartment. The individual hot water storage units are located as close as possible to taps so as to minimise the amount of water (and energy) wasted through running the hot tap before hot water is available. Shallow baths reduce water consumption and taps incorporate restrictors to reduce flow rates.

The proposal to incorporate rain water harvesting for communal refuse wash down areas, external garden taps, etc will help to reduce the building water consumption.

MATERIALS

Most of the materials to be used will be manufactured in the UK and supplied direct or through builder's merchants.

These materials will be sourced locally where feasible. All timber to be used in the construction will be sourced from renewable sources with FSC certificates. Nearest-cut lengths will be specified for timber so as to minimise waste. The following building elements will be constructed off-site; concrete floor slabs, concrete stairs, windows and doors.

DURABILITY AND FLEXIBILITY

The internal space within the apartments is adaptable, i.e. the studwork partitions can be easily demounted should future needs change.

DISABILITY ACCESS

The proposal is designed to allow for ease of access and movement for people with disabilities.

WASTE MANAGEMENT AND RECYCLING

The proposed development will include communal refuse and recycling facilities. Wherever feasible, natural raw materials and products will be used in the construction process. Any packaging such as MDF, wood or plastic wrapping will be reused to protect fragile building elements

4.0 THE DESIGN

4.5 ACCESSIBILITY AND SUSTAINABILITY

during the building process such as metallic sinks, porcelain products etc.

SOCIAL NEEDS & CRIME PREVENTION

A handover pack will be provided for the future residents, providing information about their new home including the Green Transport Plan, advice on the energy efficient appliances installed in their homes, addresses and telephone numbers for essential local services, clubs and organisations benefiting the new resident.

SUSTAINABLE PRACTICES AND STRATEGIES TO BE USED IN CONSTRUCTION

ENVIRONMENTAL SUSTAINABILITY

Pre-fabricated concrete products and, most probably, mortar silos, will be used to further reduce on site water use. Fuel use will be minimised during construction by arranging for mains power to be installed as soon as possible.

LIGHTING ASSESSMENT

In external areas low level bollard lighting will be specified to produce light that angles down to where it is most usefully needed, to avoid light pollution, and can be easily located to illuminate paths and parking areas without impacting on residents dwellings. Building lighting is proposed to be PIR controlled and set to come on from dusk until dawn, providing added security when it is needed, and reducing unnecessary energy usage. All external lighting will use low energy fittings.

VENTILATION / EXTRACTION ASSESSMENT

Domestic extract fans and ventilation (e.g. to serve kitchens and bathrooms) will be employed within the development.

Extract fans are sited to avoid visual intrusion, and noise/fumes for neighbours.

NOISE IMPACT

The building is set back from the main road and all windows are double glazed to avoid noise disturbance to residents.

5.0

SUMMARY

5.1 SUMMARY

5.0 SUMMARY

5.1 SUMMARY

This statement has set out the key principles which have influenced the detailed design of the scheme. The proposals are practical, honest, sustainable and sensitive to the surrounding area and improves the appearance and function of the site.

Use and Amount

The use of the site and the position of the buildings and open spaces have been explained and justified based on the analysis of the site and surrounding areas. The amount of development proposed has been indicated and the density shown to be appropriate for the site and wider community.

Appearance and Scale

The appearance of the buildings and spaces have been explained and it has been demonstrated that the proposal relates to its surroundings. It has also been evidenced that the scale has been influenced by the existing character of the local area.

Landscape

The hard and soft landscaping has been explained and is shown to be an appropriate response to the scheme and the local environment.

Access

It has been demonstrated that the principles of inclusive design and access for all people have been considered from the outset and informed the development process.

